



U.S. RACK,™ Inc. - 2850 Falcon Drive, Madera, CA 93637 - 559-661-3050

INSTRUCTIONS for FIFTH WHEEL RACK Model 2010-4AD

WARNING: Do NOT attempt to install or use this rack without following all instructions.

SPECIFICATIONS and SAFE LOADING REQUIREMENTS

These instructions are intended for use only upon full-size pickup trucks that have conventional fleetside bedrails, and only in combination with trailers and 5th wheel hitch set-ups that allow free rotation of the trailer without touching the Rack or its cargo. Because this Rack encroaches into the space behind the bed, carefully ensure that pivot point of your hitch is set back far enough in your bed to allow your trailer to rotate freely without striking any part of the rack or its cargo. The manufacturer cannot warrant that any combination of truck, trailer and hitch set-ups will be able to turn without striking the rack or its cargo. The safe installation and use of this Rack in combination with any trailer and any hitch set-up is the sole responsibility of the user, not the manufacturer. If you are unable to use this product safely, uninstall it and contact U.S. Rack to return it in accordance with the return policy published on our website.

This rack is designed to carry loads not exceeding a total weight of 200 lbs., the center of mass of which is located no more that half way between the front and rear crossbars. If your rack is outfitted with Cab Cushions, the overall load rating increases from 200 lbs. to 250 lbs. The load must also be spread across the width of the crossbars. It is not designed to carry loads where a vertical force is applied on either crossbar exceeding 100 lbs. (or 125 lbs. with the addition of properly adjusted Cab Cushions). This product is not warranted for use off-road or on unimproved or poorly maintained or bumpy roads. All loads must be tied down securely to the rack to prevent them from vibrating or sliding forward, backward, laterally or being blown off or broken by wind. The manufacturer does NOT warranty any automotive product and does not warranty truck bed rails against damage caused by the weight of excessive loads being applied to them when the rack is installed on a vehicle. The manufacturer is not responsible for injury or property damage resulting from the rack being improperly installed or improperly loaded, nor is it responsible for injury or property damage resulting from loads or parts of loads falling or being blown off a vehicle. Loads extending beyond the rear bumper of the vehicle must be designated with a red flag during daylight or red light during darkness in accordance with the state vehicle code.

BE SAFE: U.S. Rack is your partner in safety. Please remain alert to any abnormality or safety concern. Carrying any load can be hazardous. Make sure all parts of all loads are securely tied down against unexpected winds and vibrations caused by road hazards such as potholes. In all cases where a load extends beyond the front crossbar, use a bow line or other rope or strap, tied off securely to the front bumper or other secure location to pull the load gently down and dampen vibration that can occur during travel. Check each time you install the rack, load the rack, as well as daily to ensure that all connections are tight. Avoid roll over. As with all racks, ensure that loads are not top-heavy. Loads should be placed so that the center of mass of the load is no closer than 12” from the sides of the rack. High loads must be transported with GREAT CAUTION to prevent loads from striking low overhead objects or from tipping during turns, abrupt stops, or high winds.

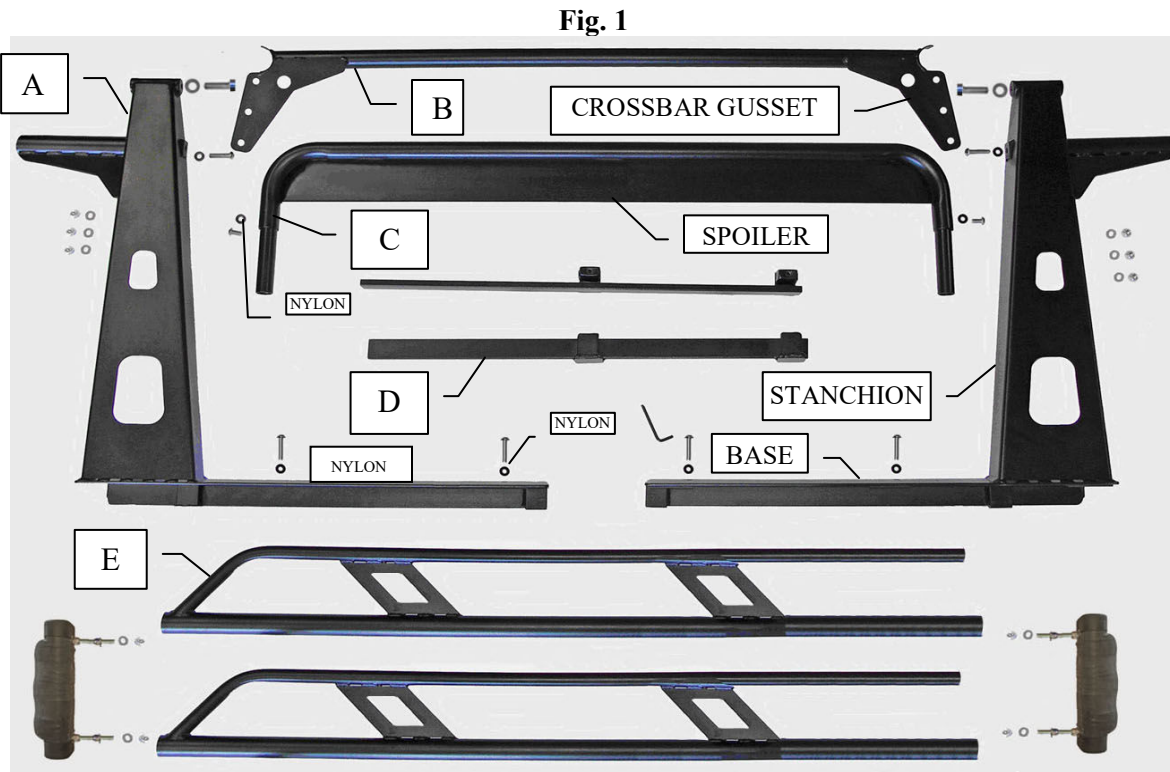
WARRANTY

This product is warranted for a period of one year against all structural defects in materials and workmanship provided that they are assembled, installed, and used in accordance with all manufacturer’s specifications and instructions. The manufacturer **cannot warrant the powder-**

coating on its products. Normal use of any powder-coated rack and exposure to weather can result in scratching of the surface, exposing metal below; therefore, maintenance on your rack will be required. To prevent rust, spot paint any scratches or breaks in the surface with a high quality metal paint. **Merchandize must be returned in the original box and packaging.** See return policies and procedures at http://www.usrack.com/merchandise_return_policy.php

INVENTORY

Your safety is paramount. Before assembling the rack, inventory and inspect all parts. Visually check each part to ensure it corresponds to the inventory list and check all welds for signs of cracking or weakness. If you do not have all the correct parts or if any parts appear to be defective, STOP! Do NOT install the rack. Contact customer service at 1-888-877-2257 to replace missing or defective parts. If you have any questions about installation, call customer service. We will be happy to help.



The Fifth Wheel Rack consists of these parts:

- A. Leg Frame (left and right)
- B. Rear Crossbar (x1)
- C. Front Crossbar (x1)
- D. Clamp Rails (left and right)
- E. Side Rails (x2)
- F. Cab Cushions (standard item on 6-ft long racks; optional item on 5-ft long racks)

Additional Hardware:

3/8-16 x 1" Button Head Cap Screws (x2); 3/8-16 x 1.75" Button Head Cap Screws (x6); 1/2-20 x 1.75" Hex Head Screws (x2); Large metal washers (x2); Small metal washers (x6); Nylon washers (x8); Nylon lock nuts (x6); Allen wrench (x1); Small tube of clear, flexible caulk (x1) **NOTE: SCREWS AND SMALL PARTS MAY ALREADY BE ATTACHED TO OTHER PARTS**

ATTENTION: If you purchased a rack with Cab Cushions see the Supplemental installation instructions at the end of these instructions.

ASSEMBLY and INSTALLATION

Read **ALL** instructions through once **BEFORE** you do anything!

Advisory on nylon lock nuts: Many nuts used in this rack are nylon lock nuts that keep screws from vibrating loose on the road. These allow screws to twist in several turns before experiencing significant resistance. You must increase torque to overcome the stiff nylon ring that the threads of the screw have to cut through the first time each screw is threaded in.



A SMALL PACKET OF GREASE SHOULD BE INCLUDED IN YOUR PACKAGE. PLACE A DAB OF GREASE ON EACH SCREW TO MAKE THREADING EASIER AND HELP PREVENT SEIZING.

- 1. Attach Leg Frame to Bedrail.** Having someone hold parts in place during the first assembly will make it easier. After you have inventoried and inspected all parts, examine Leg Frames (A) and notice that they are a mirrored pair. Select the one for the driver side and place it at the front of the bedrail of the truck as shown in Fig. 2. Examine the Clamp Rails (D) and notice that they are also a mirrored pair. Select the driver side Clamp Rail and align the treaded holes with the two holes in the base of the Leg Frame and using 2" button head cap screws with washers, install as shown in Figs 3 thru 6. Notice that the long metal rail of the clamp rail goes behind the lip of the bedrail. *Note: There are sometimes plastic clips under the bedrail. Slide the metal rail of the clamp rail in the space between them and the bedrail lip.* Install the passenger side Leg Frame in a similar manner.



Fig. 2



Fig. 3



Fig. 4



Fig. 5

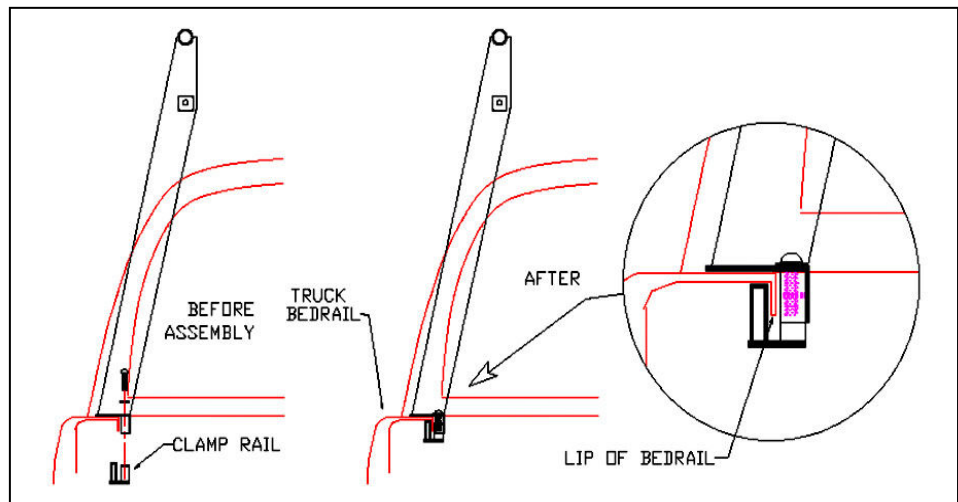


Fig. 6

2. **Install Rear Crossbar.** Examine the Rear Crossbar (B), and notice that it has a gusset plate on each end. Each gusset contains three small holes that correspond to the three threaded studs projecting from the front interior face of the Leg Frame. Align the studs with the holes and slide the studs into the holes. Install a metal washer on each stud and then thread a nylon lock nut onto each threaded stud and tighten firmly as shown in Figs. 7 thru 10.



Fig. 7



Fig. 8



Fig. 9



Fig. 10

3. **Attach Side Rails to Leg Frames.** Examine the Side Rails (E). Each has two sets of horizontal tubes projecting from toward the rear that contains threaded holes. As shown in Figs. 11 and 12, slide these tubes into the ends of the corresponding larger tubes projecting from the front of the Leg Frame. After the tubes have been pushed in as far as they will go, place black nylon washers on to 1.75" button head cap screws and thread them into the holes in the back of the Leg Frame. Tighten then the lower screw firmly and tighten the upper screw according to need. *NOTE: The upper screw allows small adjustment to the height of the rail. By tightening it more, it will raise the rail.*



Fig. 11



Fig. 12

4. **Install front Crossbar.** Examine the Front Crossbar (C) and Figs. 13 and 14 below. If you have a longer cab you may have a flat metal plate attached to it called a spoiler, which may or may not have rectangular holes cut in it for sports accessories. Notice that there are threaded holes in the bottom of the crossbar. These corresponded to holes in the bottom of the Side Rails. Insert the ends of the Crossbar all the way into the front of the Side Rails until the holes on the bottom align. Place nylon washers on the ends of two short button head cap screws and thread into the holes. Tighten both firmly with the Allen wrench.



Fig. 13



Fig. 14

5. **Caulk.** Notice the small tube of clear, flexible weather caulk. Apply bead of this in appropriate locations to prevent water from entering the interior of the rack, causing rust inside the tubes and rust water from gushing out.
6. **Safety Check.** Examine the entire rack. Ensure that it is fully tightened down and that all fasteners are tight. These fasteners should be checked frequently to ensure that they remain tight. After installation conduct a very low speed test to ensure that the trailer when attached to the vehicle turns freely even when the truck or trailer are going up or down a driveway or other dip in the grade. If you have any questions or safety concerns what so ever, contact U.S. Rack immediately for advice. When properly installed the Rack should appear as in Figs. 15 and 16, below.



WARNING

IMPORTANT SAFETY NOTES

The Fifth Wheel Rack encroaches into the space behind the truck cab. Carefully check to ensure that the trailer will not strike the cab, when turning. Carefully ensure that pivot point of your hitch is set back far enough in your bed to allow your trailer to rotate freely without striking any part of the rack or its cargo. The manufacturer cannot warrant that any combination of truck, trailer and hitch set-ups will be able to turn without striking the rack or its cargo. The safe installation and use of this Rack in combination with any trailer and any hitch set-up is the sole responsibility of the user, not the manufacturer.

The Fifth Wheel rack is strong, but it must be loaded carefully to be safe. Because of the long extension over the cab, considerable torque is applied to the Leg Frame of the rack. Because these structures are strong, the forces are transmitted to the bedrail of the truck. Excessive loads can result in damage to the bedrails of the truck or structural failure of the rack. To remain safe there are three requirements:

First, do not overload the rack. Distribute the load from left to right as much as possible and move the load toward the rear; a vertical force of no more than 100 lbs. should be applied to the front crossbar, 125 lbs. with the addition of properly adjusted Cab Cushions.

Second, tie down any load such as a kayak projecting forward of the front crossbar with a line to the front bumper to prevent the rack from vibrating up and down.

Third, consider the road and weather conditions to prevent shock. If the road is rough or the weather severe, act accordingly.

Fourth, check frequently to ensure that all fasteners remain tight.



FIG. 15



FIG. 16



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Supplemental Instructions for mounting Cab Cushions on 5th Wheel Racks

If you purchased a rack outfitted with please follow these instructions. Cab Cushions can mount under the Side Rails of the U.S. Rack 5th Wheel Rack. The purpose of the Cab Cushions is to provide supplemental support to the rack in the event of shock loading to the rack caused by pot holes or other sudden events, and to dampen sympathetic vibration that might otherwise promote metal fatigue. The Cab Cushions have a broad, padded surface and are located over the strongest parts of the cab to avoid any ordinary damage to the roof of the cab and to reduce loading on the truck bedrails during unexpected shocks.

Assembly: When you receive your Cab Cushions they should be assembled as shown in Fig. C, but these instructions will explain their assembly.

The cab cushions are an assembly with five parts as shown below in Fig. A. The Body of the assembly is a powder coated aluminum extrusion with a slot in the top. Around the Body are rubber sheaths that contain rubbery foam padding inserted below the Body. To install the hardware, as shown in Fig. B, slide the head of a carriage bolt about an inch into each end of the slot, so that the carriage bolts are about 8 inches apart. Add one metal washer and loosely thread a nylon locknut down on each carriage bolt. Slide a plastic end cap on the end of the body, until each end cap contacts the rubber sheaths and both slide under the washer and nut. Tighten the nut firmly. Thread a second nylon locknut onto the bolt so it is about half way down. The purpose of the second nylon locknut is to adjust the space between the top of the Body and the bottom of the Side Rail of the rack. One more washer should be added above Nylon Locknut 2 prior to installing the Cab Cushions on the Rack.

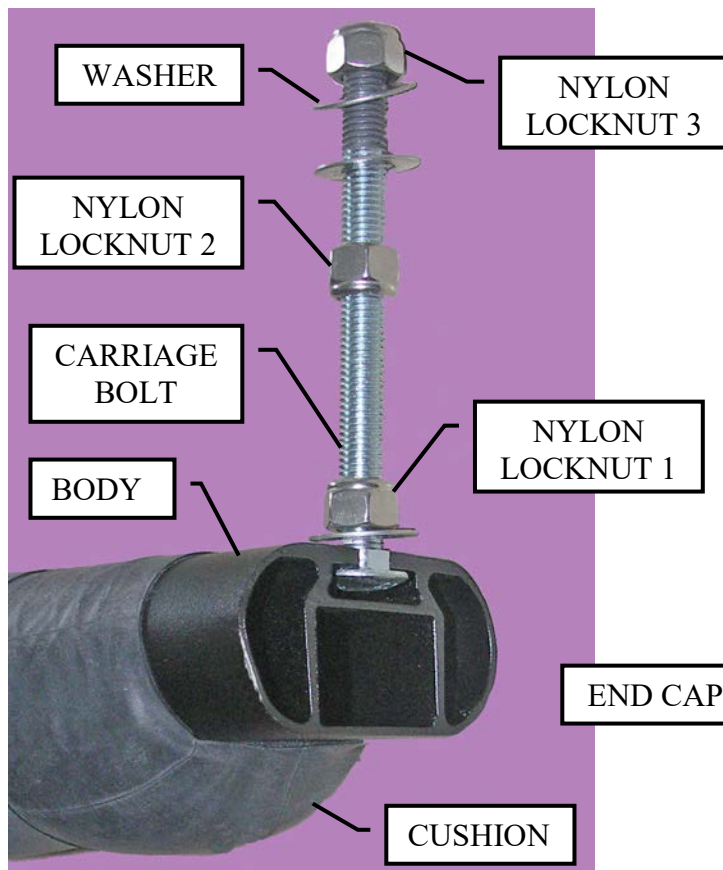


FIG. A

FIG. B



FIG. C



Installation: The larger tube of the Side Rail of the 5th Wheel Rack that is built to accept Cab Cushions has two vertical holes drilled through it. The carriage bolts of the Cab Cushions are designed to go through these holes. If they are already screwed on, remove the top nylon lock nuts and one set of washers from the top of each carriage bolt. As shown in Figures D, E, and F, align the top of each screw with the holes in the bottom of the side rails; insert the ends of screws up through holes, then place metal washer and nylon lock nuts on each and tighten firmly. Adjust the position of the nylon lock nuts if necessary to adjust height of the cushion above the cab. Adjust cushions so they rest lightly on the roof of the cab or have up to ½” clearance above the roof when the rack is loaded. NOTE: it is best to loosely install the Cab Cushions before installing the Side Rails on the Rack.



FIG. D



FIG. E



FIG. F

WARNING: The Cab Cushions are designed to help with over-stresses caused by shock loading resulting from unexpected events such as pot holes. Although this device is built with a broad padded surface, that does not mean that it can handle unlimited pounding caused by excessive speeds on rough roads. Please use caution, particularly on rough roads so as not to cause damage to the roof of your cab, or to the rack itself.