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U.S. Rack™ Inc. – 2850 Falcon Drive, Madera, CA 93637 – 1-888-877-2257

**INSTRUCTION for GALLEON OVERHEAD STAKE POCKET RACK**

**WARNING: Do NOT attempt to install or use this rack without following all instructions.**

**SPECIFICATIONS and SAFE LOADING REQUIREMENTS**

The Galleon Overhead Stake Pocket Rack is intended to carry ladders and other cargo not exceeding 500 lbs and ONLY on pickup trucks with stake pockets. **This rack is designed to carry loads, which are spread across the width of each crossbar and shared evenly between the three crossbars. It is not designed to carry loads where a force of over 100 lbs. is concentrated on any space less than 12 inches wide along any crossbar or where a force of over 200 lbs overall is loaded on either the rear or middle crossbar and where a force of over 100 lbs is loaded on the front crossbar. This product is not warranted for use off road or on unimproved or poorly maintained or bumpy roads, nor is warranted when used contrary to instructions or specified uses.** U.S. Rack does NOT warrant any automotive product and does not warrant truck bed rails against damage or failures caused by the weight of excessive loads being applied to them when the rack is installed on a vehicle. **U.S. Rack is not responsible for injury or property damage resulting from the rack being improperly installed or improperly loaded, nor is it responsible for injury or property damage resulting from loads or parts of loads falling or being blown off a vehicle.** Loads extending beyond the rear bumper of the vehicle must be designated with a red flag during daylight or red light during darkness in accordance with the state vehicle code. Ensure that neither the rack nor any cargo blocks the view of tail or brake lights from rear.

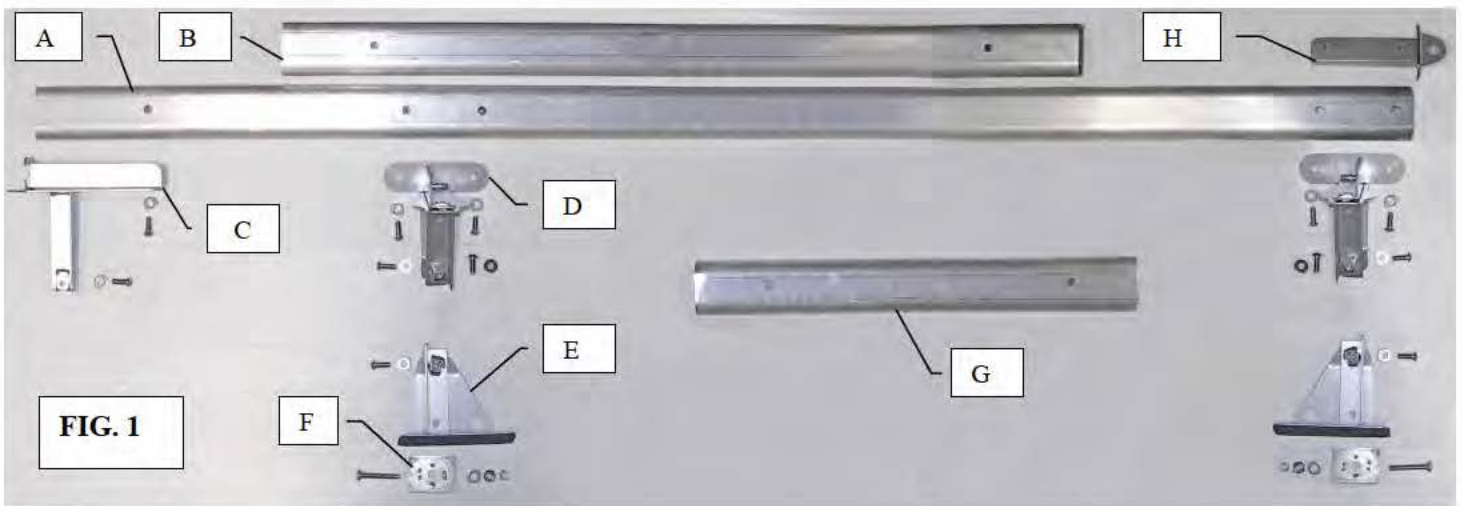
**BE SAFE: Carrying any load can be hazardous. All loads must be tied down securely to the rack to prevent them from vibrating or sliding forward, backward, laterally or being blown off or broken by unexpected wind or road hazards such as potholes. Check each time you install the rack, load the rack, as well as daily to ensure that all connections are tight. Periodically check welds for cracking caused by metal fatigue. Avoid roll over by ensuring that loads are not top heavy. High loads must be transported with GREAT CAUTION to prevent loads from striking low overhead objects or tipping during turns, abrupt stops, or high winds.**

**WARRANTY for the GALLEON RACK**

U.S. Rack™ products are warranted for a period of one year against all structural defects in materials and workmanship provided that they are assembled, installed, and used in accordance with all manufacturer's specifications and instructions. U.S. Rack **cannot warrant the powder-coating** on its products. Normal use of any powder-coated part and exposure to weather can result in scratching of the surface, exposing metal below; therefore, maintenance on your rack will be required. To prevent rust, spot paint any scratches or breaks in the surface with a high quality metal paint. **Merchandise must be returned in the original box and packaging.** See return policies and procedures at [http://www.usrack.com/merchandise\\_return\\_policy.php](http://www.usrack.com/merchandise_return_policy.php)

**INVENTORY**

**Your safety is paramount.** Before assembling the rack, lay out all the parts. Inventory and inspect all parts. Visually check each part to ensure it corresponds to the inventory list and check all welds for signs of cracking or weakness. Manufacturing and shipping mistakes can happen. If you think you have received the wrong product for your model truck, if you do not have all the correct parts, or if any parts appear to be defective, STOP and do NOT install the rack. Contact customer service at 1-888-877-2257 to replace missing or defective parts. If you have any questions about installation, call customer service. **Also, these instructions are NOT for 2000-2006 Toyota Tundra's;** they require some different parts.



- A. Side Rail (x2)  
 B. Crossbar (Front, Middle and Rear) (Front Crossbar includes flat metal Spoiler)  
 C. Front Connector (Driver side and Passenger side)  
 D. Top Connector (x4)  
 E. Base (x2 Driver side and x2 Passenger side)  
 F. Stake Pocket Insert Plate (x4)  
 G. Leg (x4)  
 H. Rear End Connector (x2)

Additional Hardware: 3/8-16 x 1" button head cap screws (x24); 3/8-16 x 2.5" carriage bolts (x4); 3/8-16 flange nuts (x4); 3/8-16 nylon lock nuts (x4); metal washers (x28); Allen wrench to fit the cap-screws

**NOTE: Some screws, washers, and nuts may be attached to other parts.**

### ASSEMBLY

Read ALL instructions through once BEFORE you do anything!



Figures below show how the parts of this rack fit together. Lay the parts out on the ground and assemble them without using any screws so you can see how they fit together.

1. **Examine the Bases.** Notice that there is a threaded hole in vertical part of the Base (E). If it contains a screw and washer, remove them for now.
2. **Examine Stake Pocket Insert Assembly.** Referring to Fig. 2, notice that the assembly contains a carriage bolt, metal washer, nylon lock nut and flange nut. Examine the Pocket Insert Plate (F) and notice that it consists of a rectangular plate with a square hole upon which a smaller plate or shape is welded. The normal configuration of this assembly is to have the smaller plate on top of the larger plate with the carriage bolt inserted from the bottom. When you receive your rack, normally the Pocket Insert Plate, washer and nuts are already attached to the Base, but if not, assemble as shown in Fig. 3. This is how the parts will appear when being inserted into the stake pocket. The nylon lock nut will not be added until later.



Fig. 2

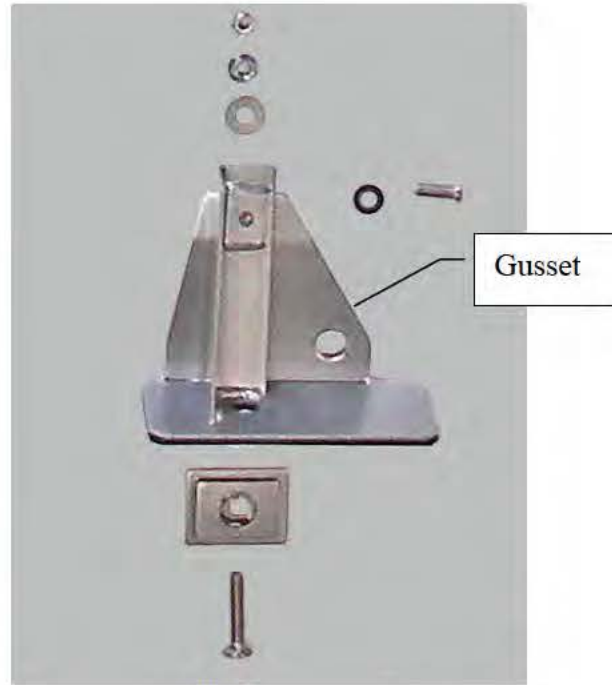


Fig. 3

#### INSTALLATION

- Install Bases.** Before installing parts, park your truck in a safe and level place. Begin installation by examining one of the Bases. Hanging from the Base by the carriage bolt should be the Insert Plate. Notice that the Base includes a vertical plate called a gusset (Fig. 3). When the Bases are installed in the stake pockets, the gussets will always be facing toward the outside of the truck bed. The eyelets in the gussets should be oriented so that they face each other (the eyelet in each of the rear bases face toward the front and the eyelet in each of the front bases face toward the rear). *NOTE: Don't be concerned about the orientation of the metal angle projecting upward. This will not be visible after the rack is installed.* Place the Stake Pocket Insert Plate down into the stake pocket as shown in Figs. 4, 5 and 6. Examine these figures carefully. Ensure that the larger portion of the Plate is fully seated below the rim of the stake pocket and the smaller raised portion of the insert seats within the profile of the rectangular hole. It is **critical** that the smaller raised portion of the Plate fits within this profile and that no edge of the larger portion of the plate is visible from above, even when the smaller raised portion of the plate is moved around within the profile of the pocket. This will ensure that when the nuts are tightened the carriage bolt and plate cannot spin, slide to one side, or come out of the pocket. **If you cannot seat the plate in accordance with these directions, STOP, and contact customer service at 888-877-2257 before proceeding with installation.** After the Pocket Insert Plate is properly seated, grasp the lock nut on the carriage bolt and continue to pull up gently to keep the Insert Plate from falling back down into the pocket while you ease the Base down onto the bed rail. Spin the flange nut down until it makes contact with the Base as shown in Figs. 7 and 8. **DON'T forget to put anti-seize grease on all threaded parts before tightening otherwise you will have trouble removing the parts later.** Tighten until the Base is firmly seated on the bedrail but not so tightly that the truck sheet metal is bent or the bolt threads are stripped. To prevent loosening, screw the nylon lock nut down onto the bolt as in Fig. 9.



Fig. 4



Fig. 5



Fig. 6



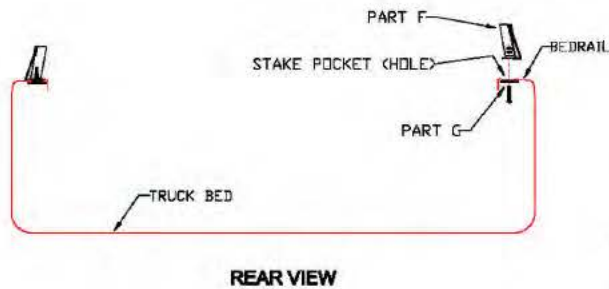
Fig. 7



Fig. 6



Fig. 9



REAR VIEW

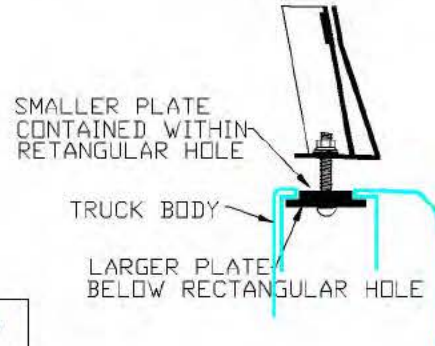
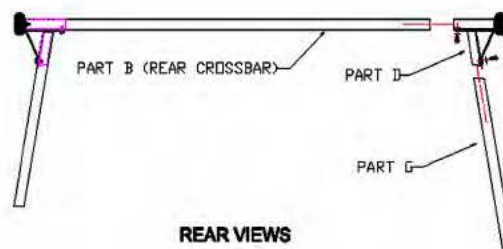
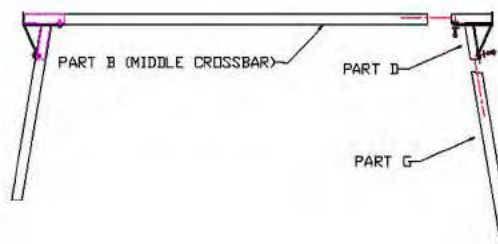
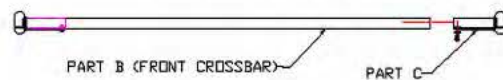


FIG. 10

4. **Attach the Crossbars to Connectors and Connectors to Legs.** Attach the crossbars (B) to the Front and Top Connectors (C), and (D) in accordance with Fig. 11. Also examine Fig. 13, which shows the overall top view of the rack. In some cases the Front, Middle and Rear Crossbars are not the same length, so if they are not the Crossbars may be marked as Front, Middle and Rear. Ensure you match the Crossbars with the correct Connectors. Begin assembly with the Front Connector and notice that there is an angular projection containing a threaded hole. Slide these projections into the ends of the Front Crossbar until the holes in the Crossbar align with the threaded holes in the connector. Place a metal washer onto a 1" button head cap screw and thread it into the hole; then tighten it firmly. Slide the appropriate projection of the Top Connectors onto their corresponding Crossbars and insert a screw with washer in a manner similar to the Front Crossbar but leave the screw loose. Do NOT tighten these screws at this time. Next attach the Legs to the Top Connectors. Attach the Legs loosely as above with a loose screw and washer.



REAR VIEWS

FIG. 11

5. **Attach the Crossbar/Leg Assemblies to the Bases.** Examine Fig. 12, below. Lower the tailgate of your truck and step up inside with the Middle Crossbar/Leg assembly. Place the bottom of one Leg over the angular projection on the Base and slide it down until the hole in the bottom of the Leg aligns with the threaded hole in the Base. To do this, it will be necessary to tilt the Crossbar so that the Leg you are attaching is aligned with the projection on the Base. Next align the bottom of the other Leg with the other Base and attach similarly. Notice that since you have not tightened the screws in the Crossbar and Legs, there is flex in the assembly which allows you to roughly align the parts and complete the assembly. After the Legs and Bases are attached, insert screws with washers into the threaded holes. Now tighten ALL of the screws firmly with an Allen wrench, but not so firmly that you strip the threads or deform the parts. Attach the Rear Crossbar/Leg assembly in a similar manner. *NOTE: In the event that you find it difficult to flex the bottom of the second Leg onto the second Base, you may loosen one or both of the bases a little to allow them to move or tip to the appropriate angle for assembly. Then before seating the Leg all the way onto the base, leave enough space to re-tighten the nuts on the carriage bolt.*

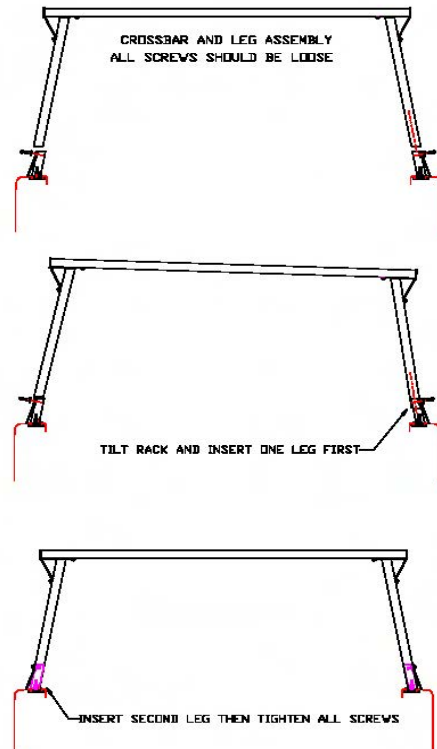
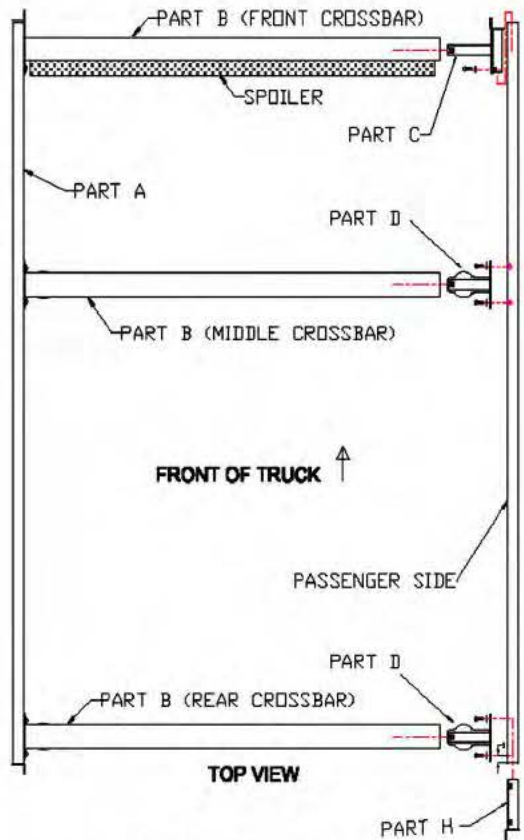


FIG. 12

6. **Attach Side Rails.** Attach the Side Rails to the rear and middle crossbars as shown in Fig. 13. First hold one of the Side Rails (A) up against the Top Connector (D) so that the holes in the mid-section of the Side Rail align with the holes in the Top Connector. Insert a 1" button head cap screw with washer into one of the holes and tighten the Side Rail loosely to the Top Connector. Move to the rear Top Connector. Insert an End Connector into the hole in the end of the Side Rail so that the holes align. Now align the holes in the Top Connector with the holes in the Side Rail. Insert a 1" button head cap screw with washer into one of the holes and loosely tighten the Side Rail to the Top Connector. Attach the other Side Rail similarly and then add additional 1" screws and washers and tighten loosely.
7. **Attach Front Crossbar Assembly.** Attach the Front Connectors to the front of each Side Rail by aligning and inserting the projections of the connectors into the open hole at the end of each Side Rail. The Spoiler is a flat piece of aluminum that is designed to reduce wind noise. The crossbar should be oriented so that the spoiler is on the bottom of the crossbar and pointed toward the rear of the vehicle. Align the holes in the Front Connectors with the holes in the Side Rails. Thread a 1" screw as shown in Fig. 13 through the hole in the exterior portion of the front connector and tighten firmly.

FIG. 13



8. **Insert and tighten all screws and nuts on the rack firmly.** Tighten firmly but not so firmly that the sheet metal or threaded parts are damaged. **Don't forget to use anti-seize grease before tightening otherwise you may have difficulty unthreading the parts later.** When fully tightened, the rack should not move in the stake pockets when pressure is applied and the rack should remain upright and stable. Frequently recheck tightness of threaded parts.



FIG. 14

Carrying any load can be hazardous. Avoid roll over. As with all racks, ensure that loads are not top heavy. Loads should be placed so that the center of mass of the load is no closer than 24" from the sides of the rack. High loads must be transported with GREAT CAUTION to prevent loads from striking low overhead objects and from tipping during turns, abrupt stops, or high winds. Loads extending beyond the rear bumper of the vehicle must be designated with a red flag during daylight or red light during darkness in accordance with the state vehicle code.